

Bradford Bypass West Environmental and Detail Design Project Update

December 2025

Project Update

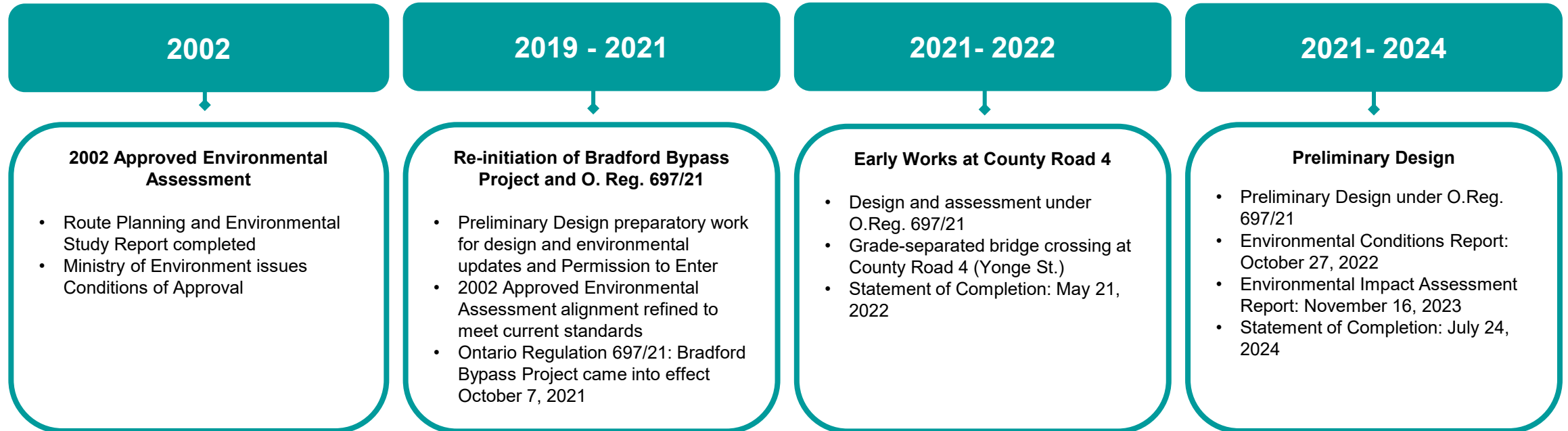
1. Project Background, Update, and Overview
2. Environmental and Detail Design Process for Bradford Bypass West
3. Detail Design of the Bradford Bypass West
4. Environmental Impacts, Mitigation Measures and Anticipated Permits and Approvals
5. Construction and Construction Staging
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A video presentation of the Bradford Bypass West Project has also been prepared as part of this Project Update and will be posted on the Project Website: <https://bradfordbypass.ca/>

Project Background, Update, and Overview

Bradford Bypass Project Background

The Bradford Bypass is a 16.3 km, controlled-access freeway between Highway 400 and Highway 404, located within Simcoe County and the Regional Municipality of York. Major project milestones include:

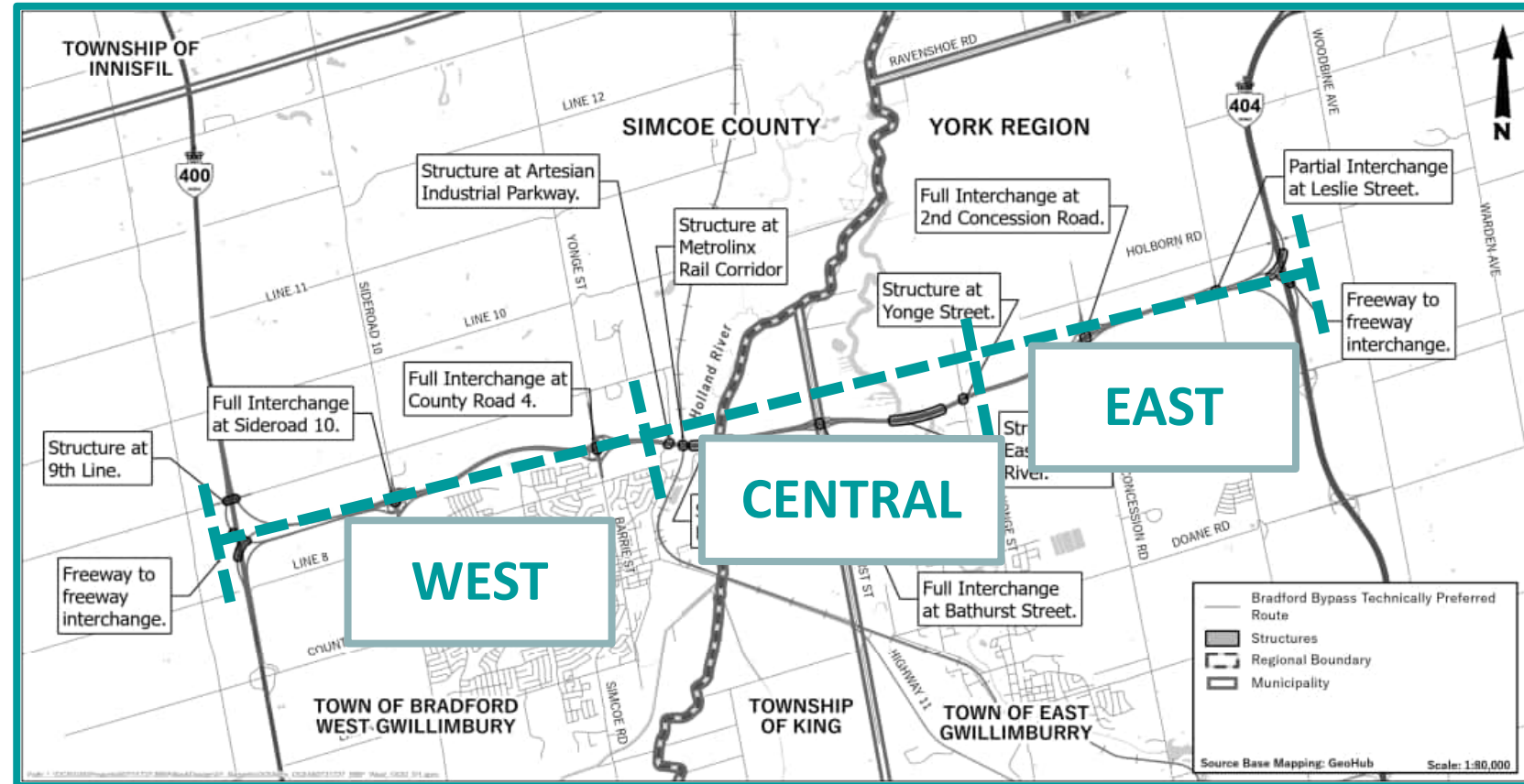


Commitments to future work, analysis and impact mitigation identified during all of the studies completed for the project are being carried forward for completion during Detail Design and construction phases.

Bradford Bypass Project Update

- Detail Design for the Bradford Bypass West was initiated in May 2024 and is being completed by AECOM.
- Contracts for the Detail Design of the central and east sections of the Bradford Bypass Project are expected to be awarded in Fall 2025.
- MTO is also in the process of retaining a Program Management Consultant to provide support in the oversight, coordination and integration of Detail Design and construction phases of all the three sections. The Program Management Consultant assignment is currently in procurement phase and expected to be awarded in the late Fall 2025.

Bradford Bypass West, Central, and East Sections



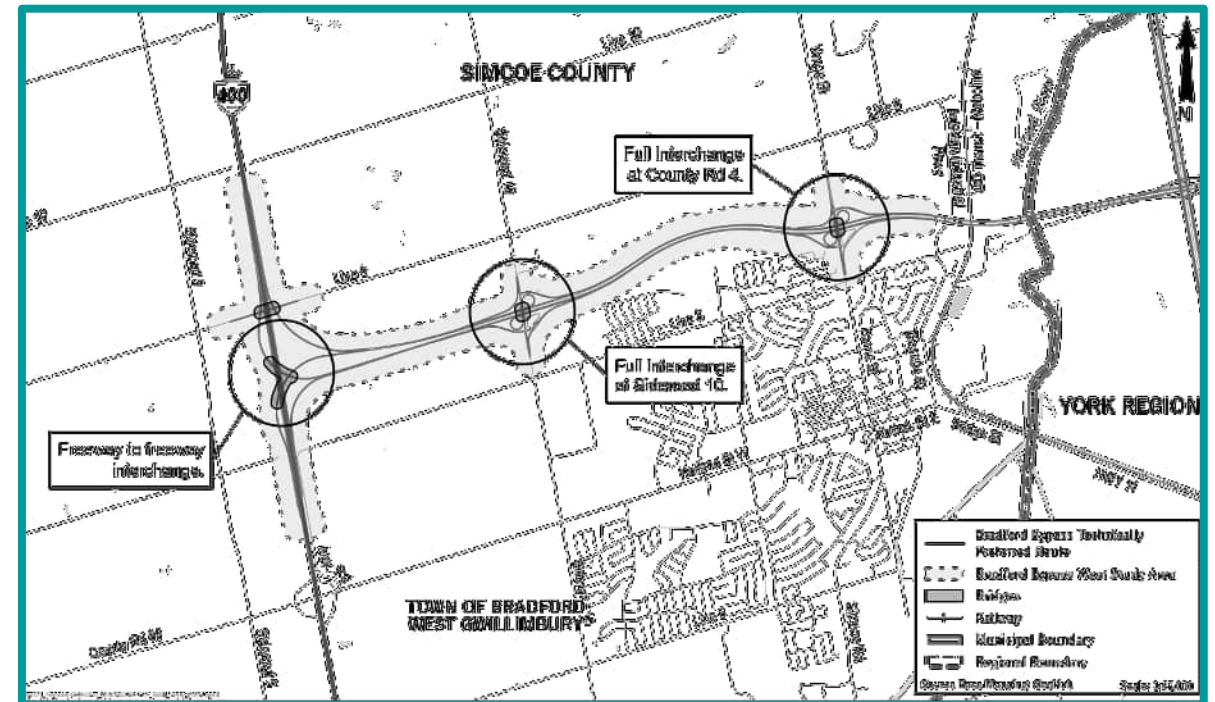
Project updates will continue to be posted to the Project Website as they are available: <https://bradfordbypass.ca/>

Overview of Bradford Bypass West

- Building on the work completed during Preliminary Design, the Detail Design work for the Bradford Bypass West was initiated May 2024.
- The Bradford Bypass West extends from Highway 400 to east of County Road 4 in the Town of Bradford West Gwillimbury.
- A Construction Manager General Contractor (CMGC) delivery model is being used for the Bradford Bypass West. This delivery model:

- ✓ Enhances design via contractor-designer collaboration
- ✓ Expedites construction of lower complexity components of the project through advance contracts – design and construction occur simultaneously, and
- ✓ Reduces risk of re-design and/or construction delay through early contractor involvement.

Bradford Bypass West Study Area



The Study Area generally reflects the Bradford Bypass West right-of-way, plus a 120 metre buffer area where environmental investigations were completed.

Environmental and Detail Design for Bradford Bypass West

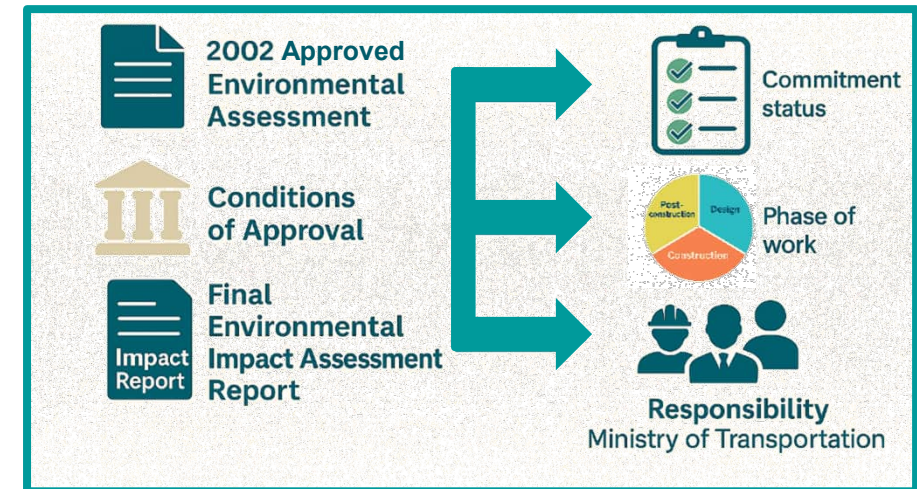
Environmental and Detail Design Work Being Completed

- The following additional environmental analysis, fieldwork and investigations are being completed to support Detail Design:
 - Terrestrial Ecosystems
 - Fish and Fish Habitat
 - Landscaping
 - Archaeology
 - Cultural Heritage
 - Fluvial Geomorphology
 - Soil and Groundwater
 - Waste and Contamination
 - Stormwater and Drainage
 - Air Quality
 - Noise
 - Agriculture
 - Land Use
 - Snowdrift
 - Human Health, and
 - Climate Change
- The environmental investigations support:
 - Impact analysis. As the design advances, environmental impacts are defined in greater detail.
 - Development of a comprehensive Landscape Design including mitigation and restoration measures such as noise walls and areas where reforestation, edge management, compensation plantings and snowdrift measures can be implemented.
 - Obtaining required permits and approvals, e.g. Species At Risk, Fisheries Act, etc.

Environmental Compliance

- During Detail Design, environmental compliance is a key consideration to determine if the project:
 - Is proceeding in accordance with applicable environmental laws, and in accordance with the Regulation (O.Reg 697/21: Bradford Bypass Project), and
 - Meets the commitments, obligations and responsibilities set forth during all previous planning phases for the project including:
 - 2002 Approved Environmental Assessment
 - Applicable conditions from the Ministry of Environment Conditions Of Approval (2002), and
 - 2023 Final Environmental Impact Assessment Report.
- An Environmental Compliance Tracking Report is being prepared at each design milestone to document:
 - Status of each commitment from previous project phases
 - Timing/phase of work in which each commitment must be met
 - Responsibility for compliance with each commitment
 - Project changes, and
 - Deliverable/report in which compliance with each commitment has been demonstrated.
- Additional environmental compliance tracking during Construction will also be required and completed by the Contractor.

Environmental Compliance Tracking



Project Changes

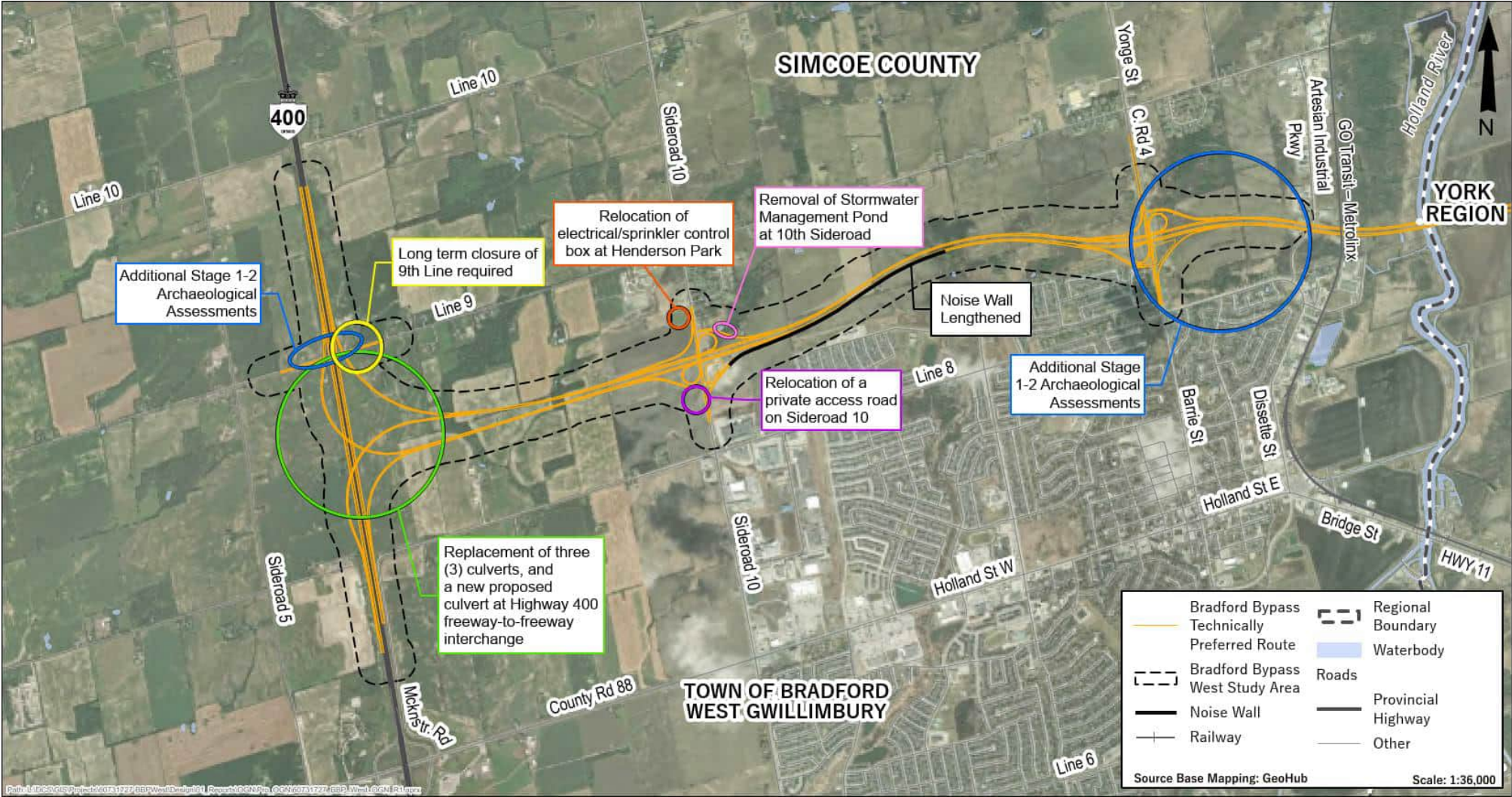
- Compliance with Ontario Regulation 697/21: Bradford Bypass Project (the Regulation) includes the need to track any changes to the Bradford Bypass Project that are inconsistent with the Final Environmental Impact Assessment Report (AECOM, 2023).
- Detail Design of the project has resulted in some changes to the project as summarized in the table below and shown on a map on the next slide.

Project as described in the Environmental Impact Assessment Report	Change to the project identified during Detail Design
Three (3) culverts (1350mm, 1500mm and 1650mm) on Highway 400 identified for extension	Two (2) culverts (1350mm and 1500mm) on Highway 400 will now be replaced
Drainage design within the Highway 400 and Bradford Bypass Freeway to Freeway Interchange	New culvert introduced within the Highway 400 and Bradford Bypass Freeway to Freeway Interchange
No impacts to Henderson Soccer Field	Utility relocations on Sideroad 10 require temporary impact to the Soccer Field
A private access on Sideroad 10 identified as displaced	Location for reinstatement of the private driveway has been identified
All Stage 1 and 2 archaeological assessment identified as complete	Additional Stage 1 and 2 assessments have been required in support of other changes such as the temporary impacts to the Soccer Field

- All changes identified through the Bradford Bypass West Environmental and Detail Design Project so far have been assessed and determined to be **“Not Significant”**, and will be reflected on the Project Website.

Location of Project Changes

The assessment of environmental impacts and mitigation measures presented in later slides includes impacts associated with these project changes.



Impact Assessment of Project Changes

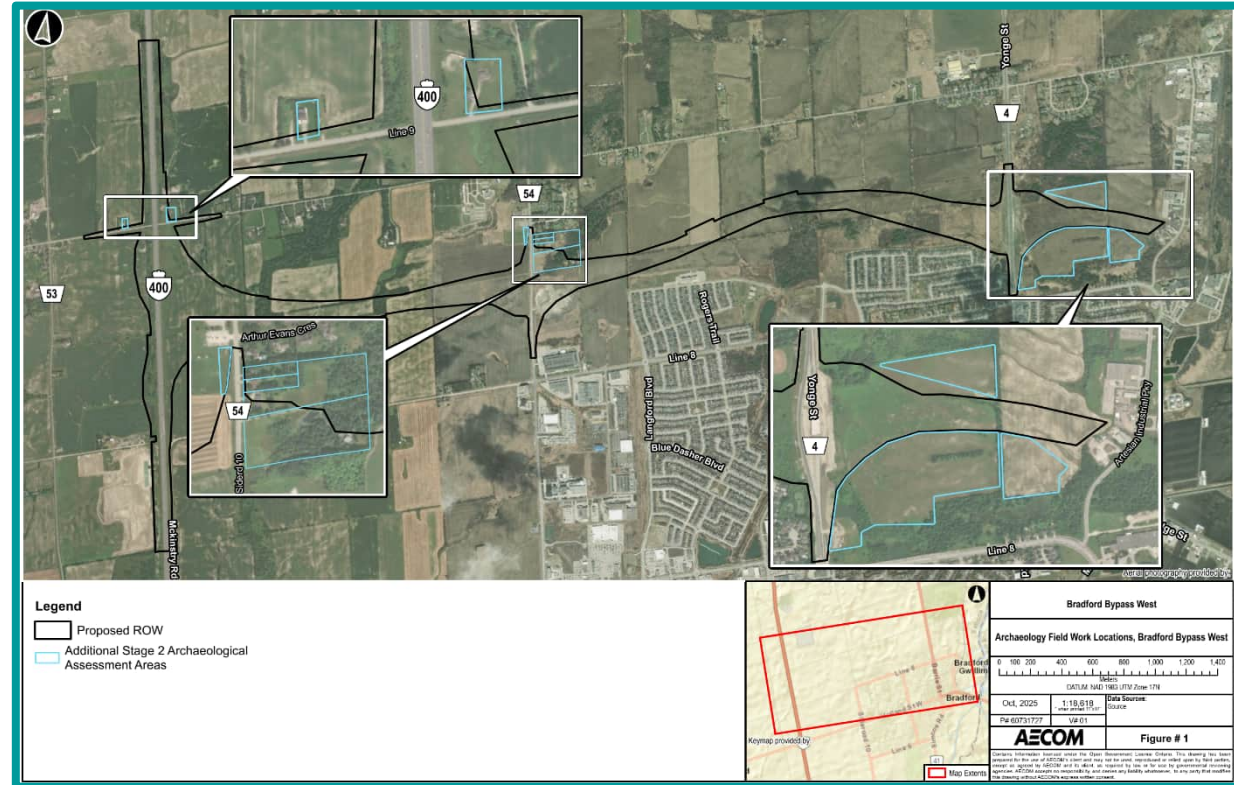
CHANGE TO THE PROJECT					
FACTOR AREA	Replacement (instead of extension) of two (2) culverts on Highway 400	One (1) new culvert at Highway 400 Interchange	Relocation of a private access road on Sideroad 10	Relocation of Electrical/Sprinkler Control Box at Henderson Park	Removal of Stormwater Management Pond at 10 th Sideroad
Natural Environment	No new impacts.	No new impacts.	Minor impact.	No new impacts.	No new impacts identified as the stormwater management pond is now being removed from the Bradford Bypass West design.
Social Environment			The relocated access will cross an existing drainage ditch. Mitigation will include minor adjustments to the drainage system in the immediate vicinity of the access.	No features of natural sensitivity within existing Soccer Field.	
Cultural Environment			No new impacts.	Minor temporary impact to Soccer Field to relocate utility line.	
	All culverts are located within the right-of-way identified during Preliminary Design and therefore all impacts have been assessed.	All culverts are located within the right-of-way identified during Preliminary Design and therefore all impacts have been assessed.	The relocated access is located with Town right-of-way for Line 8. All impacts were previously assessed and mitigated during construction of Line 8.	Impacted area will be restored and use of the Soccer Field is not anticipated to be impacted.	
				No new impacts.	
				Stage 1-2 Archaeological Assessment completed and the area has been cleared of archaeological potential during Detail Design.	

An **Addendum is being prepared** and will be made available on the Project Website.

Impact Assessment of Project Changes

- Additional Stage 1, 2 and 3 Archaeological Assessment have been completed.
- The additional work has been carried out to complete impact assessment of Project Changes where needed.
- The additional archaeology work has also been completed to support development of Construction Staging Plans.
- Areas where additional archaeological work has occurred during Detail Design are shown below.
 - A small area of Henderson Soccer Field
 - Two (2) areas on 9th Line where building demolitions will require the use of heavy machinery
 - Three (3) areas east of County Road 4 where MTO owned property may be used as storage and laydown areas during construction.

Location of Additional Stage 2 Archaeological Assessment along Bradford Bypass West (AECOM, 2025)



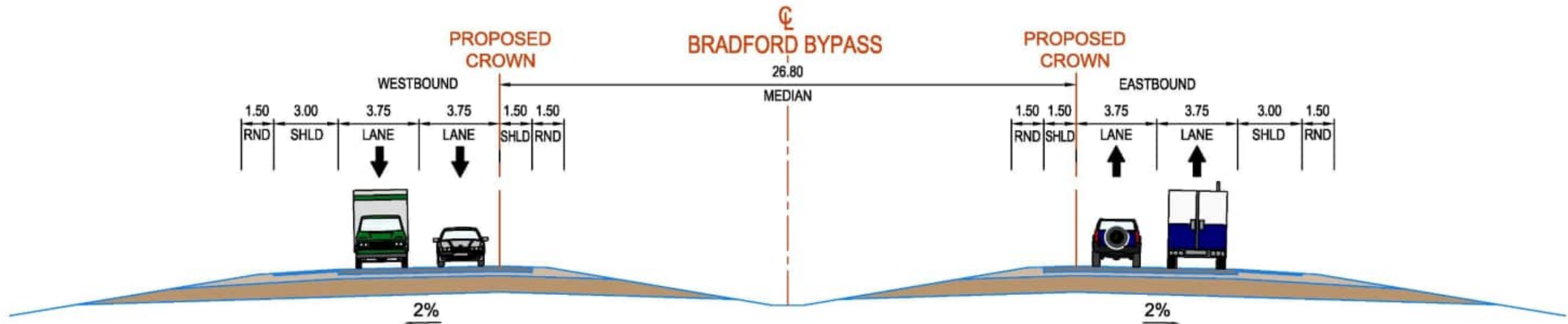
An Addendum Memo will be available on the Project Website with further details of these Project Changes.

Detail Design of Bradford Bypass West

Mainline Design

- The Bradford Bypass West will be implemented in an interim configuration of 4-lanes, with 2 general purpose lanes in each direction.
- The ultimate cross section for the Bradford Bypass is an 8-lane controlled access highway, with a dedicated High Occupancy Vehicle (HOV) lane.
- The ultimate cross section will be implemented by widening into the median, resulting in no change to the right of way.

Interim Configuration of Bradford Bypass West (AECOM, 2025)



- Interchanges will be located at Sideroad 10 and County Road 4.
- A freeway-to-freeway interchange will connect Bradford Bypass to Highway 400.

Crossing Roads and Active Transportation

Crossing Roads and Active Transportation Elements



- Crossings of the Bradford Bypass West will be provided at Sideroad 10 and County Road 4, and crossings of new freeway interchange ramps will be provided at 9th Line.
- Crossings include active transportation elements consistent with municipal transportation planning and active transportation objectives including:
 - Paved shoulders for active transportation users on both sides of 9th Line
 - A boulevard on Sideroad 10 to accommodate a multi-use path to be implemented by the Town in the future, and
 - Reconstruction of the existing multi-use path on Country Road 4 where impacted by construction of Bradford Bypass West.

Carpool Lot

- A carpool lot will be constructed at County Road 4 within the Bradford Bypass West interchange.
- The carpool lot includes:
 - 115 parking spaces
 - Barrier free parking spots, and
 - Future electric vehicle charging spots.
- The design accounts for future transit expansion, including the addition of bus bays.

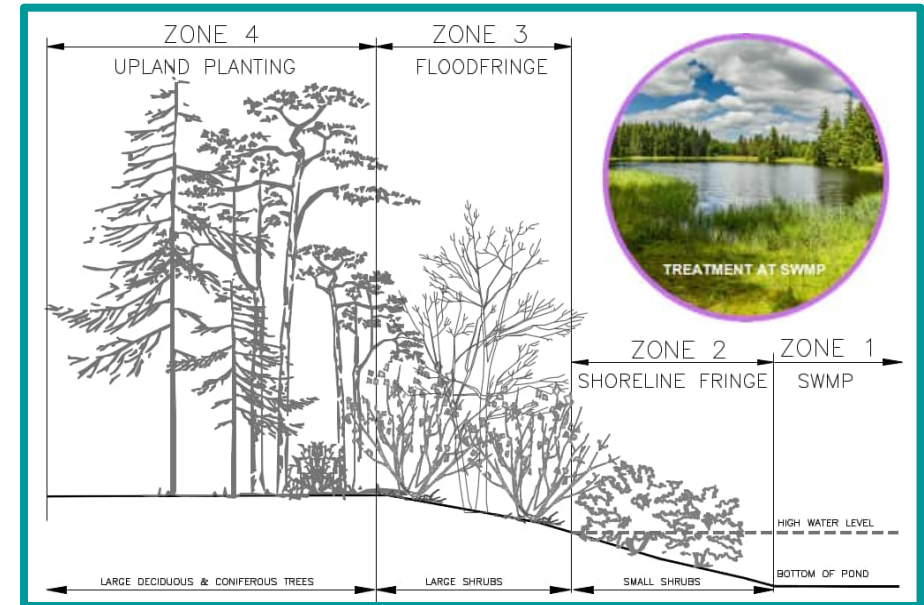
Example of an Existing Carpool Lot at Highway 401 and Brock Road (Whitby GO Station) (AECOM, 2025)



Drainage and Stormwater Design

- Drainage and stormwater management design has been completed in accordance with the Ministry of Environment, Conservation and Parks design standards and guidelines.
- Design achieves or exceeds provincial criteria and local conservation authority guidelines for quality and quantity controls.
- Elements of the drainage and stormwater management design include:
 - Replacement and/or extension of culverts under Highway 400 and 9th Line
 - New culverts to provide drainage across the Bradford Bypass West and the new highway ramps at the proposed interchanges
 - Side ditches and flat-bottom grassed swales/enhanced grassed swales
 - Four new stormwater management ponds and relocation of two existing ponds, and
 - Erosion and sediment control measures.
- Design is closely coordinated with fisheries, fluvial, terrestrial, groundwater, and landscape teams to ensure a holistic approach which minimizes impacts to the environment is developed.

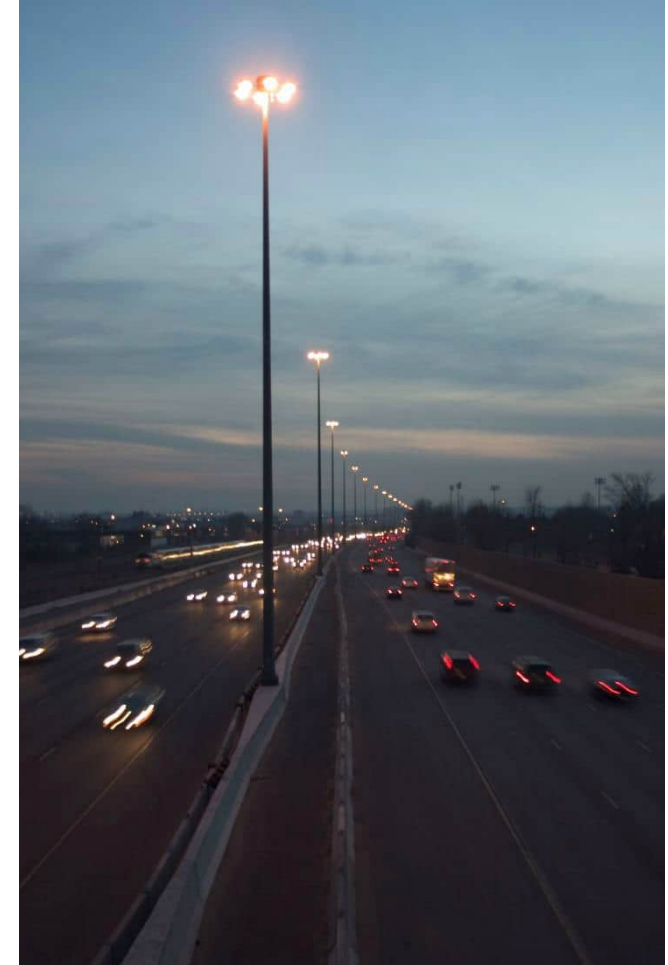
Stormwater Management Pond Typical Cross Section (AECOM, 2025)



Traffic Signals and Lighting

- Traffic and electrical design has been completed to determine locations and requirements for:
 - Lighting:
 - Full illumination will be provided at the freeway-to-freeway interchange, and
 - Partial illumination will be provided at Sideroad 10 and County Road 4.
 - Traffic Signals:
 - Signals will be located at the ramp terminals on Sideroad 10 and County Road 4.
 - Closed Circuit Television.
 - Variable Message Signs.

Example of lighting along Highway 401, looking west from Harwood Avenue (AECOM, 2025)



Environmental Impacts and Mitigation Measures

Environmental Impacts and Mitigation Measures

Fish and Fish Habitat

- Construction of the Bradford Bypass West will result in impacts to fish and fish habitat.
- In consultation with the Department of Fisheries and Oceans (DFO), a Fisheries Act Authorization is being pursued to address these impacts.
- Offsetting measures are being developed in consultation with DFO.
- Proposed offsetting measures include:
 - Habitat restoration and enhancement (natural channel design)
 - Habitat creation where possible and by adding more channel length, seasonal wetted areas or ponds
 - Fish passage improvements, and
 - Riparian plantings.

Channel flowing through a forest feature, facing upstream (AECOM, 2024)



Environmental Impacts and Mitigation Measures

Terrestrial Environment

- Impacts to terrestrial features will include vegetation removal, disturbing or removing potential faunal habitat and floral species.
- Butternut and Bat Species At Risk have been confirmed as present in the Study Area of Bradford Bypass West.
- Approval under the provincial amended Endangered Species Act (2007) is being pursued, during the interim period before the Species Conservation Act (2025) takes effect.
- Measures have been developed to mitigate impacts to species at risk in consultation with the Ministry of Environment, Conservation and Parks.
- Measures have been included in the Landscape Design and include:
 - Edge management plantings adjacent to natural heritage features
 - Restoration of impacted areas through plantings of native tree and shrub species
 - Use of specific seed mixes which reflect adjacent uses and natural heritage features
 - Replacement of Species At Risk Habitat.

Fresh – Moist White Cedar Coniferous Forest Type (FOC4-1) vegetation community within the Bradford Bypass West Study Area (AECOM, 2025).



Environmental Impacts and Mitigation Measures

Hydrogeology and Groundwater

- Groundwater monitoring wells have been placed throughout the Bradford Bypass West Study Area to confirm hydrogeologic conditions.
- Conditions are being documented in Hydrogeological Investigation Reports which will also include clear requirements for management of groundwater during construction to minimize impacts to groundwater quality and quantity:
 - Based on known conditions in the Study Area, a water taking approval is anticipated to be required.
- Review and update / additions to the Water Well Surveys completed during Preliminary Design is also ongoing.
- A Construction Dewatering Plan will be developed by the Contractor to include details of where and when groundwater will be encountered and require management.
- The Construction Dewatering Plan will include a protocol in the event public concerns about well interference are provided to the Contractor or MTO.

Borehole Investigations and Monitoring Well Installation (AECOM, 2025)

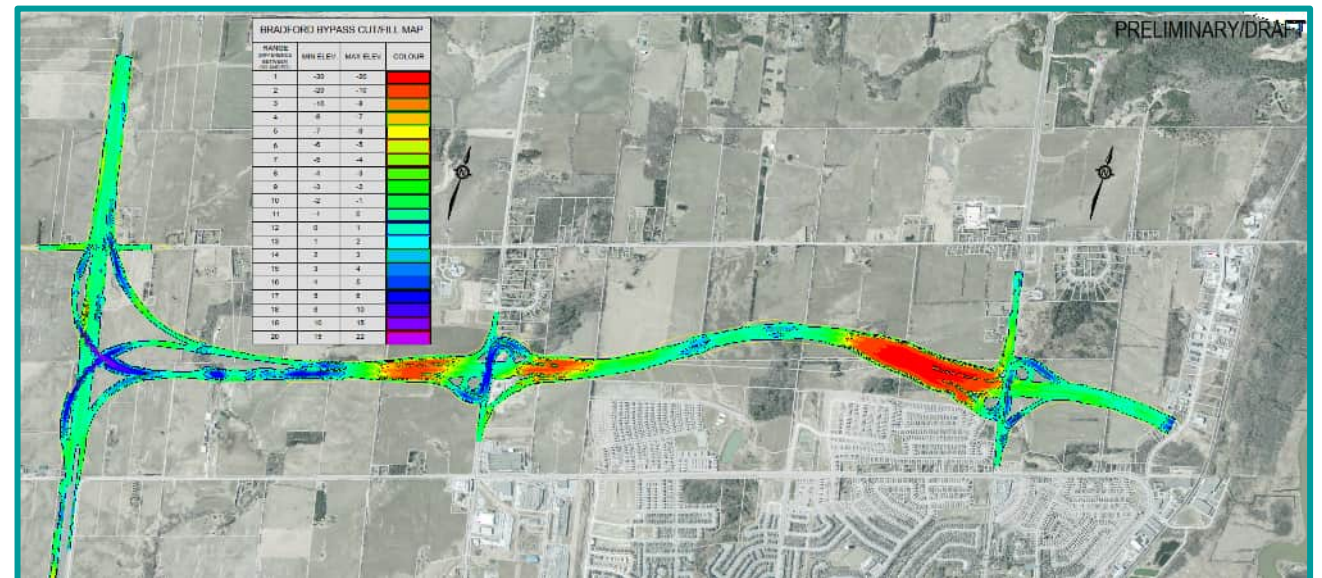


Environmental Impacts and Mitigation Measures

Waste and Contamination

- Investigations throughout the corridor have taken place to confirm soil and water quality.
- Additional investigations have also confirmed locations where designated substances or hazardous materials may be encountered.
- Design of the Bradford Bypass West requires more excavation (cut) of soil than areas of fill and a large volume of surplus soil is expected to be generated.
- When suitable, surplus materials will be re-used on future MTO projects including the Central and East sections of the Bradford Bypass.
- Materials unsuitable for reuse will be retrieved, handled, transported, and disposed of in accordance with the appropriate regulations and standards.
- An Earth Management Plan is also being prepared and will include design, monitoring, and maintenance requirements for the surplus soils which will be stockpiled on MTO property within the Study Area.

Excavations (red/yellow) and Fills (blue/purple) required to construct the Bradford Bypass West Section (AECOM, 2025)

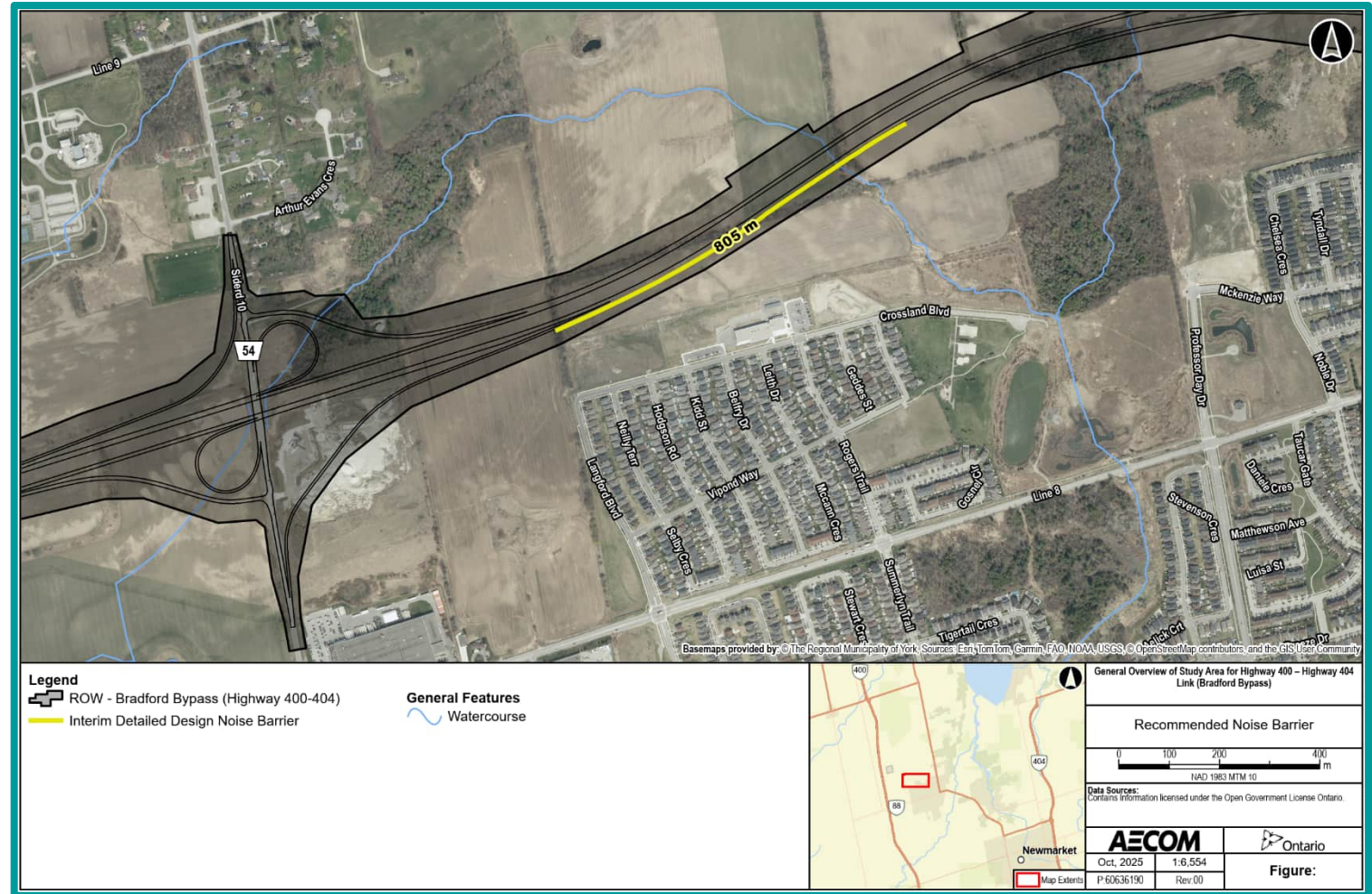


Environmental Impacts and Mitigation Measures

Noise

- The Traffic Noise Impact Assessment completed during Preliminary Design was reviewed and an update was completed using the Detail Design road geometry and information on noise barriers installed by residential developers within the Study Area.
- The noise assessment confirmed feasibility of a noise wall on the north side of Line 8 surrounding Rogers Trail consistent with the EIAR is suitable for the interim traffic conditions and 4-lane scenario.
- A Construction Noise Report has also been prepared to identify mitigation measures that minimize noise effects during construction.
- The Construction Noise Report also includes a protocol in the event public concerns about noise are provided to the Contractor or the MTO during construction.

Location of Noise Barrier along Bradford Bypass West (AECOM, 2025)



Environmental Impacts and Mitigation Measures

Air Quality and Human Health

- A Screening Level Human Health Risk Assessment was completed to evaluate the potential health impacts of increased emissions arising from the Bradford Bypass.
- The assessment examined a 500m Study Area around the Bradford Bypass and considered the effects from traffic use in areas adjacent to the project in addition to the effects of the Bradford Bypass Project.
- The results indicated that, even under worst-case conditions, most airborne emissions of contaminants from the Bradford Bypass would not result in an unacceptable increase in health risk to communities surrounding the proposed corridor.
- To minimise potential effects to the extent possible, landscaping is recommended, where feasible, within MTO's right of way.

Agriculture

- A review of the Preliminary Design Agricultural Impact Assessment Report was completed to determine if any updates to the impacts or proposed mitigation measures were warranted as a result of the Bradford Bypass West Detail Design.
- The review concluded that there were no changes in the project footprint since Preliminary Design, and therefore no additional impacts to agricultural resources are anticipated.

Snowdrift

- A review of the Preliminary Design Snowdrift Analysis Report was completed to determine if any updates to the snowdrift modelling, snowdrift impacts, and proposed mitigation measures were needed as a result of the Bradford Bypass West Detail Design. It was determined that no updates were required.
- Measures to minimize snowdrift are included in the Landscape Design.

Environmental Impacts and Mitigation Measures

Cultural Heritage

Built Heritage Resource: View of the rear tree line and field to be impacted by Bradford Bypass West (AECOM, 2025)



- Building on the Cultural Heritage Evaluation Reports prepared during Preliminary Design, a Heritage Impact Assessment (HIA) was completed for a Built Heritage Resource (BHR 5) within the Study Area for Bradford Bypass West.
- Impacts are limited to the displacement of a tree line and area of agricultural field at the rear of the property with no impacts to the buildings associated with this heritage resource.
- Mitigation of impacts has been proposed with reinstatement of the existing conditions along the new property line proposed as part of the Landscape Design.

Environmental Impacts and Mitigation Measures

Archaeology

- Stage 4 Archaeological Assessments at the following sites within Bradford Bypass West have been completed during Detail Design and to ensure compliance with the Stage 3 Archaeological Assessments completed during Preliminary Design:
 - Wheatfield Site (BaGV-113)
 - Bradford Ridge Site (BaGv-115)
- In addition, Stage 1, 2, and 3 Archaeological Assessment has been completed for the Sutherland Wesleyan Methodist Cemetery to clearly define the limits of the Cemetery and avoid impacts during construction of the freeway-to-freeway interchange.
- Stage 1, 2 and 3 Archaeological Assessments have also been completed for additional MTO property adjacent to the Bradford Bypass West right of way and where ground disturbance is anticipated as buildings are demolished, storage of construction materials potentially occurs or where mitigation measures are being implemented.

Archaeology Stage 2 Test Pitting



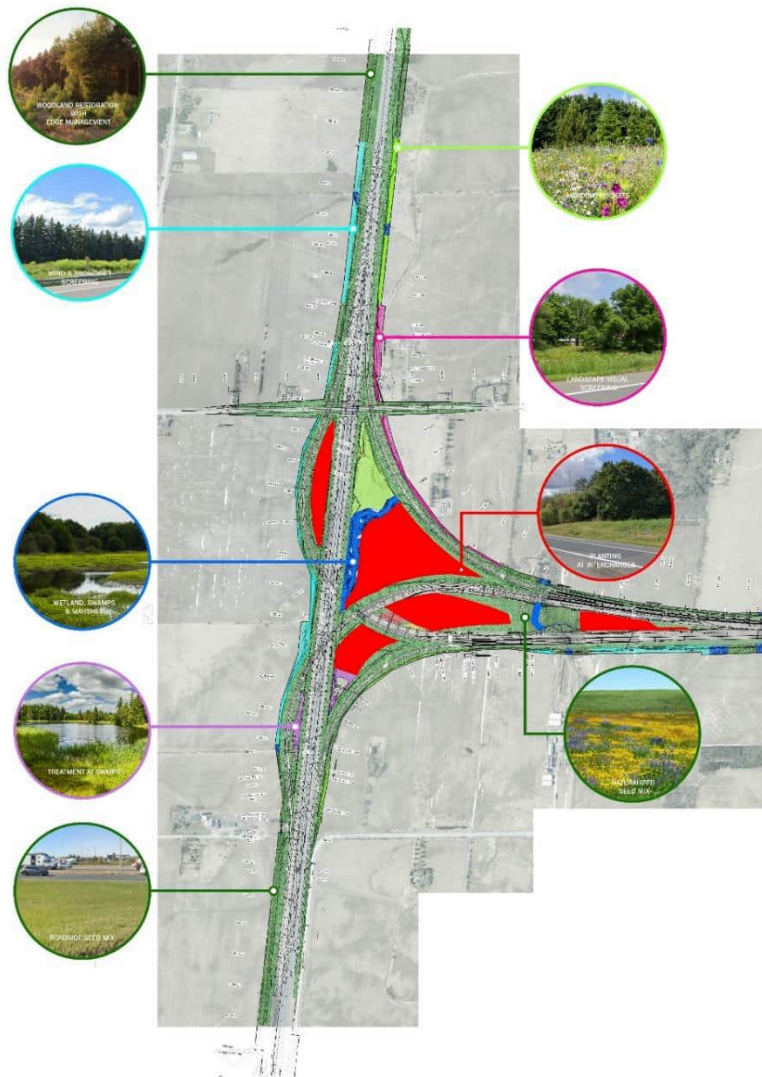
Environmental Impacts and Mitigation Measures

Landscape Design

- A comprehensive Landscape Design including mitigation and restoration measures such as noise walls and areas where reforestation, edge management, compensation plantings and snow drift measures can be implemented.
- Key elements to developing the Landscape Design have included:
 - Field investigations to confirm existing conditions
 - Offsetting measures and recommendations to mitigate impacts to fisheries throughout the corridor
 - Measures have been developed to mitigate impacts to Species at Risk, and
 - Coordination with all environmental disciplines to delineate Environmentally Sensitive Areas and identify areas for:
 - Vegetation and site restoration
 - Landscape designs and plantings have been recommended in consideration of adjacent land uses including heritage resources and urban areas, and
 - Wildlife crossings and Landscape Design elements which encourage wildlife movement via structural openings and culverts.

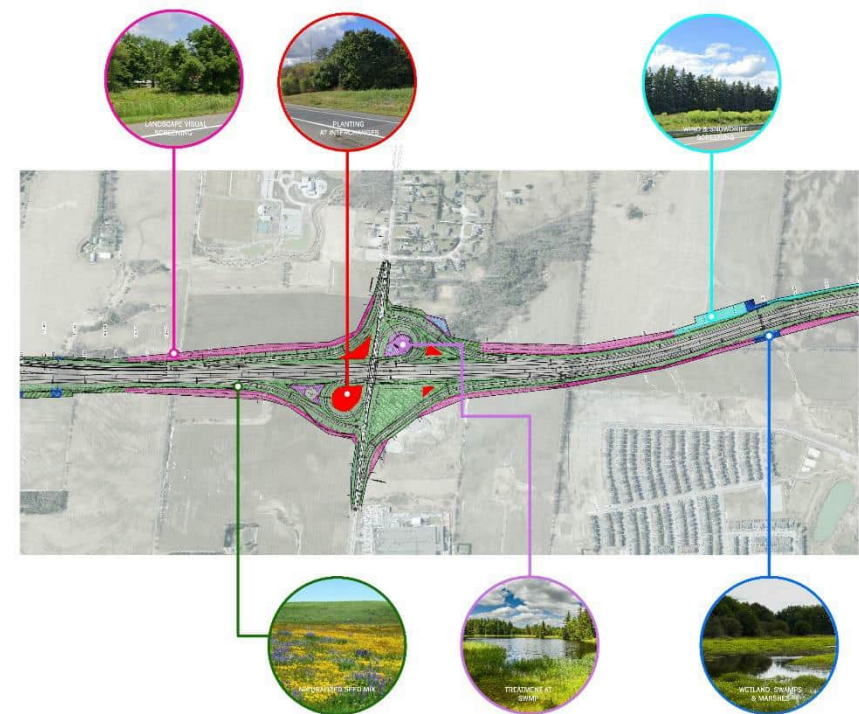
Landscape Design – Highway 400 to Sideroad 10

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- LEGEND**
- ROW
 - RESTORATION OPPORTUNITIES:**
 - WETLANDS, SWAMPS & MARSHES
 - WOODLAND RESTORATION WITH EDGE MANAGEMENT
 - MEADOWS/THICKETS
 - SWIM PONDS
 - LANDSCAPE ENHANCEMENT TREATMENTS:**
 - PLANTING AT INTERCHANGES LARGE DECIDUOUS AND CONIFEROUS TREES WITH SHRUB PLANTING AT THE EDGES
 - LANDSCAPE VISUAL SCREENING
 - WIND & SNOWDRIFT SCREENING
 - SEEDING

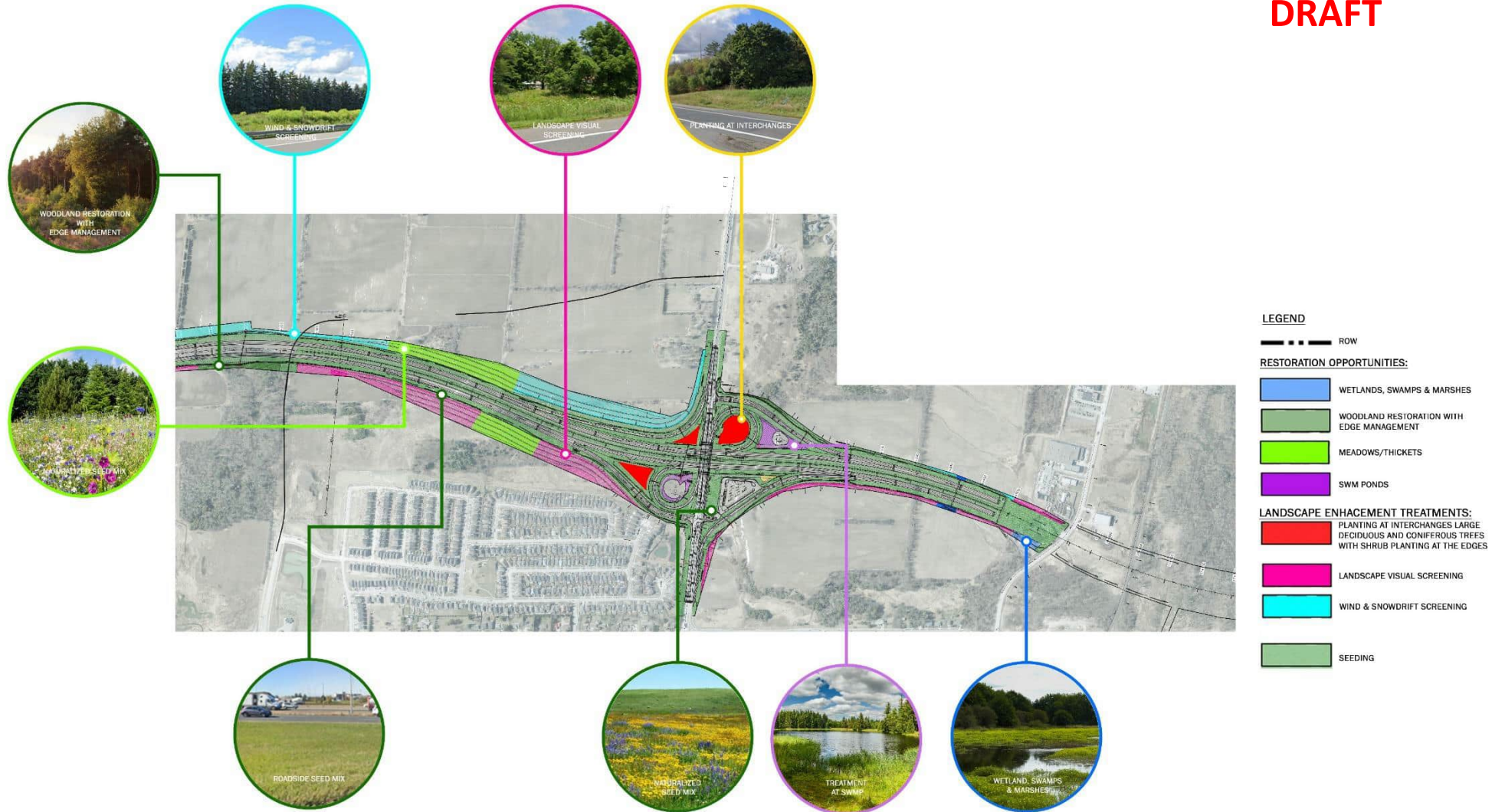
- LANDSCAPE TREATMENTS PLAN VIEWS:**
- WOODLAND RESTORATION WITH EDGE MANAGEMENT**
MIX OF LARGE MATURE TREES AND CONIFEROUS TREES WITH UNDERSTORY TO MATCH EXISTING FOREST
 - WIND & SNOWDRIFT SCREENING**
COMBINATION OF LARGE AND MEDIUM SIZE CONIFEROUS TREES AND LARGE SHRUBS PLANTING
 - LANDSCAPE VISUAL SCREENING**
COMBINATION OF LARGE AND SMALL DECIDUOUS TREES WITH CONIFEROUS TREES AND SHRUBS
 - WETLANDS, SWAMPS & MARSHES**
COMBINATION OF CONIFEROUS AND SMALL DECIDUOUS TREES WITH SHRUBS AND HERBACEOUS PLANTING GROUPS



- LEGEND**
- ROW
 - RESTORATION OPPORTUNITIES:**
 - WETLANDS, SWAMPS & MARSHES
 - WOODLAND RESTORATION WITH EDGE MANAGEMENT
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 - LANDSCAPE VISUAL SCREENING
 - WIND & SNOWDRIFT SCREENING
 - SEEDING

Landscape Design – County Road 4

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Environmental Permits and Approvals

- The Project Team is consulting with agencies to confirm the permitting requirements for the project. Anticipated permits include:

Agency / Approval Authority	Permit / Approval
Fisheries and Oceans Canada	Temporary Crossings of Watercourses
Fisheries and Oceans Canada	Fisheries Act Authorizations
Ministry of the Environment, Conservation and Parks	Endangered Species Act Approval*
Environment Canada	Schedule 1 Migratory Birds Convention Act
Ministry of the Environment, Conservation and Parks	Ontario Water Resources Act Registration
Bereavement Authority of Ontario	Cemetery Investigation Authorization
Ministry of Citizenship and Multiculturalism	Archaeological Concurrence and Acceptance of Reporting
Ministry of Transportation	Environmental Clearance for Initial Vegetation Removals; Sideroad 10 Temporary Detour and Bradford Bypass West

*the Species Conservation Act, 2025 was enacted as part of Bill 5 (Protect Ontario by Unleashing our Economy Act, 2025), and should it come into force, would replace the Endangered Species Act, 2007.

Construction of Bradford Bypass West

Construction Manager General Contractor Model

- A CMGC delivery model is being used for Bradford Bypass West.
- Under this delivery model, Detail Design and construction can occur simultaneously as components of the project advance and environmental clearances are issued.
- The CMGC Model:
 - Enhances design via contractor-designer collaboration
 - Expedites construction of least complex components of the project, i.e. design and construction occur simultaneously, and
 - Reduces risk of re-design and/or construction delay through contractor involvement.
- The CMGC model has allowed for construction of Bradford Bypass West to begin with work split into 3 stages.

Construction Stages

Fall 2024 – Initial Vegetation Removals

- By Fall 2024, environmental investigations and design had advanced to allow for initial vegetation removals.
- This work was carried out in late Fall.

Fall 2025 – Sideroad 10 Temporary Detour

- Construction of a temporary detour of Sideroad 10 to minimize traffic impacts and allow for construction of the Sideroad 10 bridge.
- Work began Summer 2025 to relocate utilities.
- Design includes environmental protection measures to be employed during construction to protect the environment.

Spring 2026 – Bradford Bypass West

- Construction of the Bradford Bypass West is anticipated to begin in Spring 2026.
- Design of Bradford Bypass West summarized in this Project Update includes environmental protection measures to be employed during construction.
- Environmental protection measures are required during all construction stages.
- These measures protect the environment and delineate areas where work / entry is not permitted as a result of property limits, proximity to sensitive environmental features or where a permit is needed before work begins.

Erosion and Sediment Control Fence
Protection of Adjacent Watercourse



Erosion and Sediment Control Fence
delineating the Work Area



Construction Staging - Traffic

- Construction staging plans have been developed and maintain existing traffic patterns and access, where possible.
- Staging plans have been developed in consultation with municipal and regional staff as well as Emergency Service Providers.
- Local access to private properties will be maintained during construction.
- Where closures are required, advance notices of lane closures will be provided.

9th Line Closure and Detour

- Due to the various structures to be constructed at 9th line, a multi-year closure and detour of traffic on 9th Line is required:
 - Local property access will be maintained.
 - Traffic will be detoured via 10th Sideroad, County Road 88, and 5th Sideroad.
 - Traffic analysis has been completed and the closure / detour is not anticipated to have significant impacts on traffic operations in the surrounding road network.
 - Signal timing plans at the intersections along the proposed detour route will be optimized.
 - Truck turnarounds have been accommodated at 9th Line for winter plowing operations, garbage collection, etc.
 - Detour signage will be provided.

Highway 400 Closures

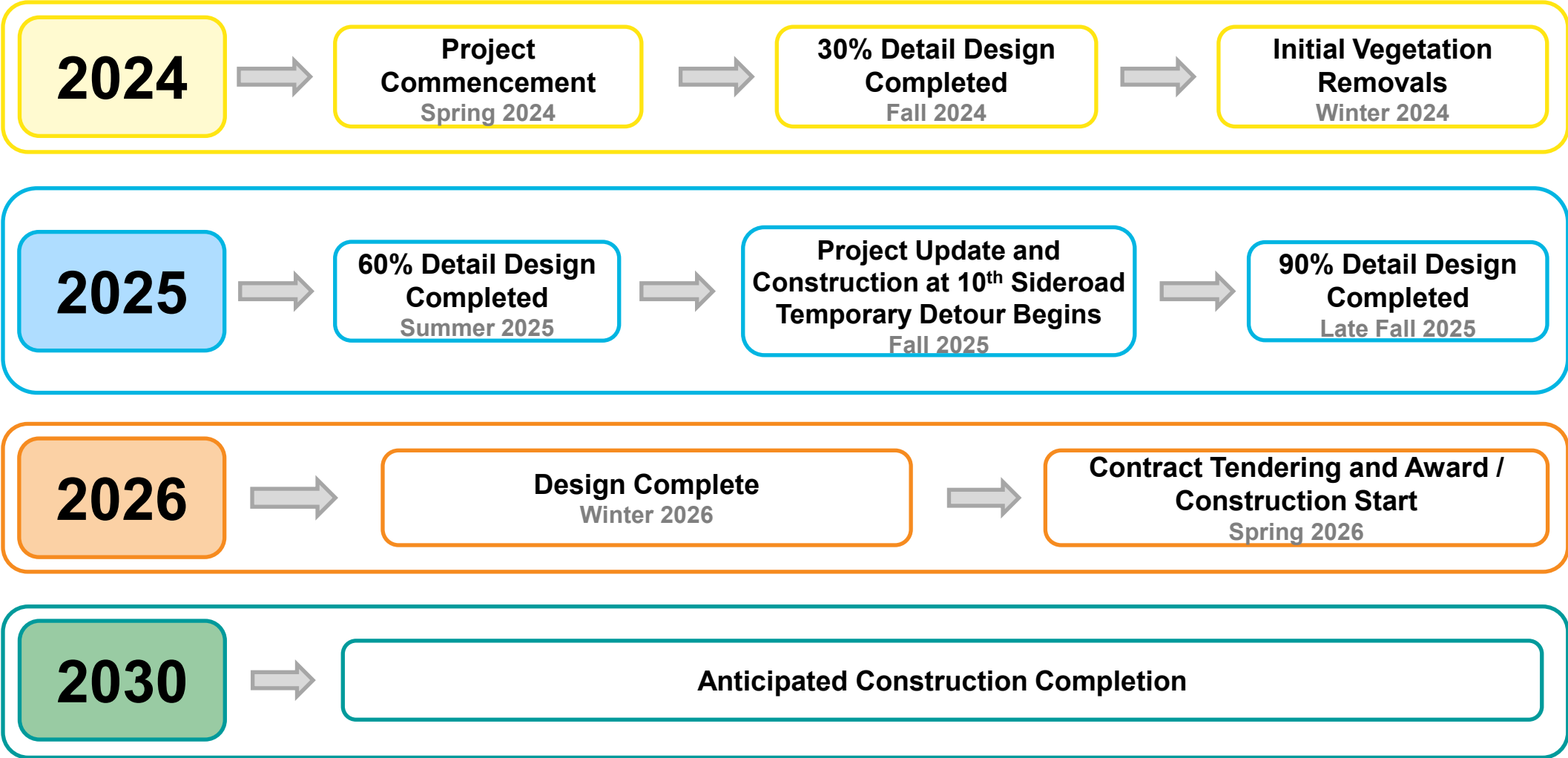
- Due to structural works taking place directly above Highway 400, a full closure of the highway is required:
 - Four (4) closures in each direction are anticipated.
 - Each closure will occur at night for approximately five (5) hours.
 - Advance notification of the closures will be provided.

9th Line Closure and Detour



Schedule and Next Steps for Bradford Bypass West

Bradford Bypass West Project Schedule



Sign up to receive all project updates via the Project Website: <https://bradfordbypass.ca/>

Thank You

A **video presentation** of the Bradford Bypass West Detail Design will be available on the Project Website as part of this Study Update.

Please Contact Us with any comments or requests for additional information.

We are committed to providing accessible services as set out in the Accessibility for Ontarians with Disabilities Act, 2005. If you have any accessibility needs, please contact our Accessibility Coordinator as soon as possible by emailing projectteam@bradfordbypass.ca. If you require documents in formats other than conventional print, or if you have specific accommodation needs, please let us know so we can make arrangements in advance.

Comments are being collected to assist the MTO in meeting the requirements of the *Ontario Environmental Assessment Act*. This material will be maintained on file for use during the study and may be included in Project documentation. Information collected will be used in accordance with the *Freedom of Information and Protection of Privacy Act*. With the exception of personal information, all comments will become part of the public record.

Telephone



1-877-247-6036

Email



projectteam@bradfordbypass.ca

Project Website



www.bradfordbypass.ca

Next Steps



- Following this Study Update, the Project Team will:
 - Incorporate feedback received into the Bradford Bypass West Detail Design
 - Continue engineering design, fieldwork and environmental studies, including development of impacts and mitigation measures, and
 - Complete fieldwork, investigations and the Detail Design of the Bradford Bypass West.

Thank you!